

Overflight Working Group



Devils Slide Rock Recommendations



Working Group

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Sarah Allen, Ph.D.--Marine Scientist, National Park Service

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Andy Wilson--Pilot and Representative of California Pilots Association

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Working Group Technical, Logistical and Facilitator Support

Marina Piscalish-- Mediator/Facilitator, Center for Collaborative Policy

Sage Tezak--NOAA

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Charter

...consider and to **make recommendations** regarding the **locations and dimensions** of areas where the elevation of motorized aircraft should be regulated to **minimize potential disturbance** to birds and marine mammals.

...stimulus...came from the Farallones Sanctuary staff to **follow up on numerous comments** received during regulatory proceedings **over several years...**

Process

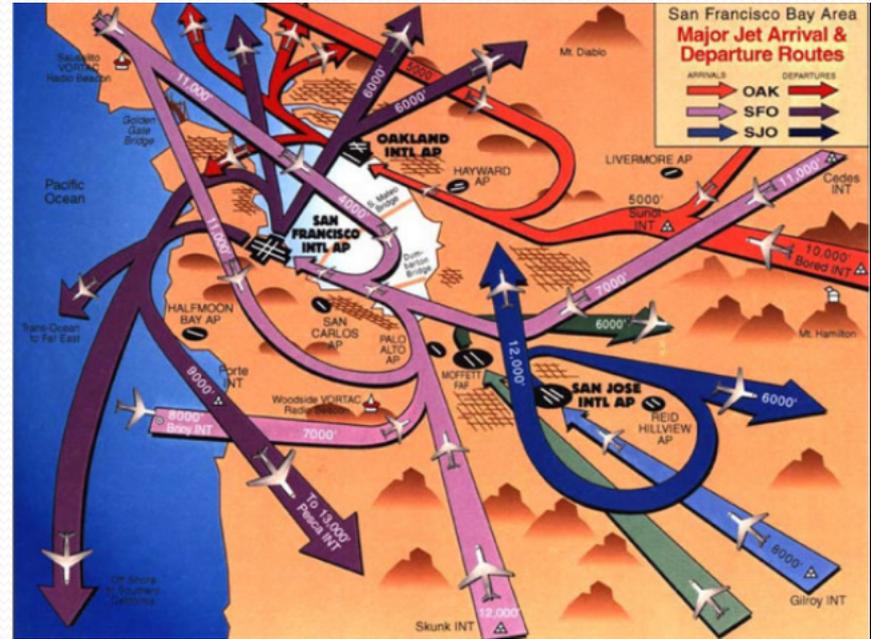
- 13 months
- Six all day meetings
- Site visits
- Conference calls

Results

- 39 page report, site by site changes suggested
- Adopted by GFNMS at January SAC meeting

FAA manages airspace, but will listen

Devils Slide Rock



Specific DSR Recommendations

- 1) Request the FAA to change chart markings to make pilots aware of Devil's Slide Rock and risks to resources:

- Create marking on the chart that combines [two approaches currently in use over the Lawrence Livermore Laboratory and Alameda Air Station](#). Use a Magenta Circle and insert a text box that explains that it is a "sensitive nesting area" and "request 1000 Ft. AGL."

Sensitive Nesting Area. Pilots are requested to avoid flight below 1000 Ft. AGL in this area. See Supplement.

A mock-up of this warning on the FAA charts is linked [here](#).

- If NOAA would like support to address this with FAA, US Fish & Wildlife Service and others can be asked to write letter of support or even attend meeting, if thought to be helpful.
- 2) Immediately engage with FAA's current process to re-consider Class B Airspace to better support this need. i.e., ask FAA to